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**DEVELOPMENT
PROPOSAL
SUMMARY
& URBAN DESIGN
BRIEF**

**CORKTOWN COOP
200 FOREST AVENUE
HAMILTON**

**RESIDENTIAL
DEVELOPMENT**

01 // introduction

1.1 PURPOSE AND OBJECTIVES OF THE STUDY

High quality neighbourhood principles and practices are key to designing, implementing, and achieving neighbourhoods that are active, vibrant, and sustainable in the long term. The purpose of this Urban Design Brief is to demonstrate how the proposed infill development in the Corktown Area is in keeping with current best practices and principles in neighbourhood design. Together with the Planning Justification Report that outlines the land use planning merits of the proposed development, this Urban Design Brief is prepared in support of the Zoning By-law Amendment (ZBA) application that are submitted for the subject site.

This Urban Design Brief examines how the proposed development, at 200 Forest Ave and 221/223 Charlton Ave E (200 Forest Avenue), fits within the existing built form context of the area, meets the intent of relevant City of Hamilton urban design policy and guideline documents, and incorporates principles of good urban design.

SITE CONTEXT:

The subject site is located within the property of 200 Forest Avenue in the Corktown Neighbourhood of Hamilton. Located on the north side of Charlton Avenue East, it is generally bounded by Aurora Street to the west, Ferguson Avenue on the West and Forest Avenue to the north. The subject property is the site of an existing surface parking lot operated by the Corktown Co-op Housing Corporation, plus 221 and 223 Charlton Avenue E.

RELEVANT STUDIES:

This report has been prepared with consideration for the following supporting policies, reports, and documents:

- Urban Hamilton Official Plan Urban Design Policies;
- City of Hamilton Site Plan Guidelines;
- Urban Hamilton Official Plan: Cultural Heritage Resource Policies;
- Architectural Drawings, Prepared by Romanov Romanov Architects

This Urban Design Brief examines how the proposed development, at 200 Forest, fits within the existing built form context of the area, meets the intent of relevant City of Hamilton urban design policy and guideline documents, and incorporates principles of good urban design.

1.2 PROJECT DESCRIPTION

Corktown Co-operative Homes is proposing to construct a new 3-storey (10.2m) residential building fronting on Charlton Avenue. This proposed new building will be in addition to the retained 4-storey, 51 unit apartment building fronting on Forest Avenue, and will contain a total of 17 dwelling units: 4 units will be located within the at-grade 'townhouses', while 13 seniors apartments will be provided within the first, second and third storeys. Access to the seniors apartments will be provided via an entrance setback from the Charlton Avenue front property line, while the 'townhouse' units will have exclusive access directly from Charlton Avenue. The site will contain a total of 68 residential units.

One (1) level of below grade parking is proposed, accommodating 43 spaces for the residential units and without visitor parking spaces. Ingress and egress will be provided along Charlton Avenue. Two-way vehicular access will be provided from Charlton Avenue along the east end of the building frontage.

The parking access will also be open through to the rear of the building, providing views to the common garden. The site plan renderings, along with floor plans and elevations, are available in Section 3-4 of this document.

1.3 DESIGN INTENT

The intention of the 200 Forest Avenue development is to create a multi-family mixed tenure co-op housing development and enhanced built form and street level experience for this block. The proposed project will introduce a unique development format and urban expression to the neighbourhood through the design of residential units geared towards families and seniors in the same building. The project is designed to reference elements of the surrounding heritage character while providing a contemporary expression of urban form. The overall approach is to introduce a coherent residential building of modest floor plate, occupying approximately a quarter of the site, with an increased outdoor amenity open space, reducing the overall proportion of mass to the development area. The building is composed of a lower four unit 2-storey 'townhouse' component, and transitioning to 3 levels of seniors apartments. The building design and layout is focused towards enhancing the pedestrian experience at-grade, through the introduction of an internal open space to the rear of the development, and reinforcing the Charlton Avenue address through tight setbacks and no front yard parking.

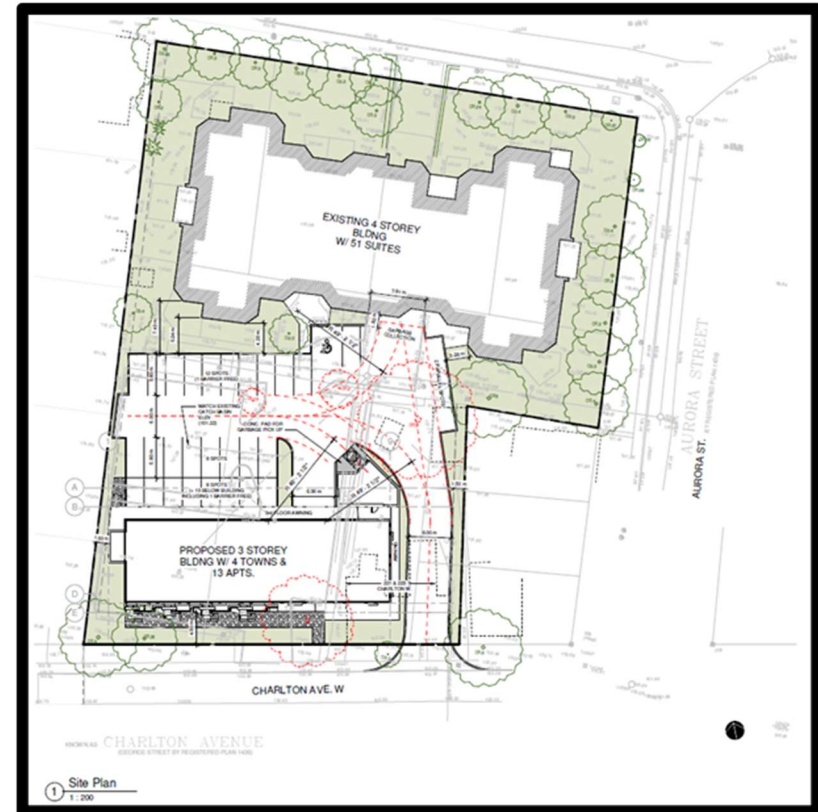
The streetscape will be enhanced by careful landscaping, featuring preservation of existing trees, soft and hard treatments, and provisions for direct opening of views towards open space.

Setbacks are maximized to the south, east, and west providing adequate distance between this project and the existing 4-storey residential building to the south, and future development. To reduce the development impacts onto the properties along the western property line, a setback is included, which approximately aligns with the existing setback condition provided between the 4-storey building to the north

of the site and the properties along Ferguson Avenue. In addition, the building steps back at the third floor along the Charlton Avenue frontage to reduce the street level impacts.

Between the proposed building and the existing 4-storey building to the north, an open space has been provided for enhanced ground level pedestrian activity and social interaction, creating a safer street level environment.

In summary, the project at 200 Forest Avenue attempts to integrate coherent, rational infill development with a site-sensitive street treatment featuring diversity of form and tenure, material and texture, carefully considered landscaping, transparency, and impacts to the surrounding context.





View west along Chariton Avenue towards Downtown Hamilton

02 // physical context

2.1 THE SUBJECT PROPERTY

LOCATION

The subject area of the site to be redeveloped is located on the north side of Charleston Ave E, mid-block between Aurora Street and Ferguson Avenue. The property has a frontage of approximately 55 metres along Charlton Ave E and 69.5 metres along Forest Ave, with a depth of approximately 68 metres, and a total site area of approximately 5,062.4 square metres. The property area to be developed is currently used generally as a surface parking area. Three medium-sized street trees line the Charlton Avenue frontage.

2.2 NEIGHBOURHOOD CONTEXT

The subject property is located within the southeast edge of Corktown, considered one of Hamilton's oldest neighbourhoods. Situated north of the Niagara Escarpment, the neighbourhood is located just outside the heart of Hamilton's established downtown core, but has lost a great deal of its early housing stock with a few remaining pockets of historic 2-storey houses mixed with newer mid-rise apartments. In general, the area includes a mix of employment, institutional, 2-3 storey residential buildings, with a number of mid-to-high-rise apartment buildings set against the backdrop of the Niagara Escarpment to the south. (See Figure 1) The area surrounding the site encompasses an eclectic range of land uses, including:

- 207, 209, 211, 213, and 215 Ferguson Avenue S directly to the west make up five 2-storey rowhouses built in 1886-1887; with 207, 211, 213, and 215 Ferguson Ave S listed in the City of Hamilton Heritage Register Volume 1: List of Designated Properties and Heritage Conservation Easements, 209 Ferguson Ave S listed within the City of Hamilton Heritage Register Volume 2: Inventory of Buildings of Architectural and/or Historical Interest, and 207, 209, 211, 213, and 215 Ferguson Ave S listed in the City of Hamilton Heritage Register Volume 3: Canadian Inventory of Historic Building (CIHB);
- The Queen Victoria Elementary School on the west side of Ferguson Avenue;
- Low-rise residential structures to the east along Aurora Avenue;
- 4-storey apartment building to the north along the northern half of the subject property and located at 200 Forest Avenue; and,
- Varied residential building forms, ranging from a 6-storey residential apartment and 2-storey low-rise residential houses south facing the property, along the Charlton Avenue frontage.

Notwithstanding the designated features and heritage character of the built form along Ferguson Avenue, much of the building fabric in the area exhibits a wide range in age and architectural styles.

A number of other notable amenities/features are located in close proximity to the site, including the Corktown Park, the Niagara Escarpment, the former high level Ferguson Avenue Pumping Station built in 1912, and the Queen Victoria Secondary School to the west along Ferguson Avenue. The area offers a glimpse into the historic past of Hamilton with the heritage facades along Ferguson Avenue, natural areas and parks, and the Niagara Escarpment, which provides one of Hamilton's most important mountain-to-bay linkages for pedestrians and vehicular traffic.

Figure 2 illustrates the built form and uses that are located within the immediate vicinity of the site and their associated building heights while Figure 3 is a collection of photographs illustrating buildings and streetscapes the surrounding area.

NORTH

Directly north of the subject site re-development area is the existing 4-storey apartment building to be retained, and wrapping along the majority of the Forest Avenue frontage. Just west of this building are 2-storey residential structures listed within City of Hamilton Volume 1:List of Designated Properties and Heritage Conservation Easements, Volume 2: Inventory of Buildings of Architectural and/or Historical Interest, and Volume 3: Canadian Inventory of Historic Building (CIHB).

SOUTH

Lands directly south of the subject site are occupied by a 6-storey residential building, fronting on the south side of Charlton Avenue.

Just west of this building are three 1-2 storey residential buildings, followed by the Niagara Escarpment embankment further east.

The properties further to the south and adjacent to the northern

edge of the Niagara Escarpment area, are occupied by a row of 2.5 storey residential buildings and the former Ferguson Avenue Pumping Station, also listed within City of Hamilton Volume 1: List of Designated Properties and Heritage Conservation Easements.

EAST

The properties directly to the east of the subject site include 7 low-scale 1-storey residential houses. Further to the east, on the facing block, are a range of 2-storey older single family residential buildings.

WEST

Lands directly to the west of the subject site include a row of 2-storey townhouses built in 1886-1887 listed within City of Hamilton Volume 1:List of Designated Properties and Heritage Conservation Easements, Volume 2: Inventory of Buildings of Architectural and/or Historical Interest, and Volume 3: Canadian Inventory of Historic Building (CIHB). Along the western frontage of Ferguson Avenue is the Queen Victoria Elementary School. Further west, one block along Charlton Avenue there is a range of existing built forms, including 2-3-storey single family residential buildings, and a range of mid-to-high-rise apartment buildings set within a higher density apartment neighbourhood context

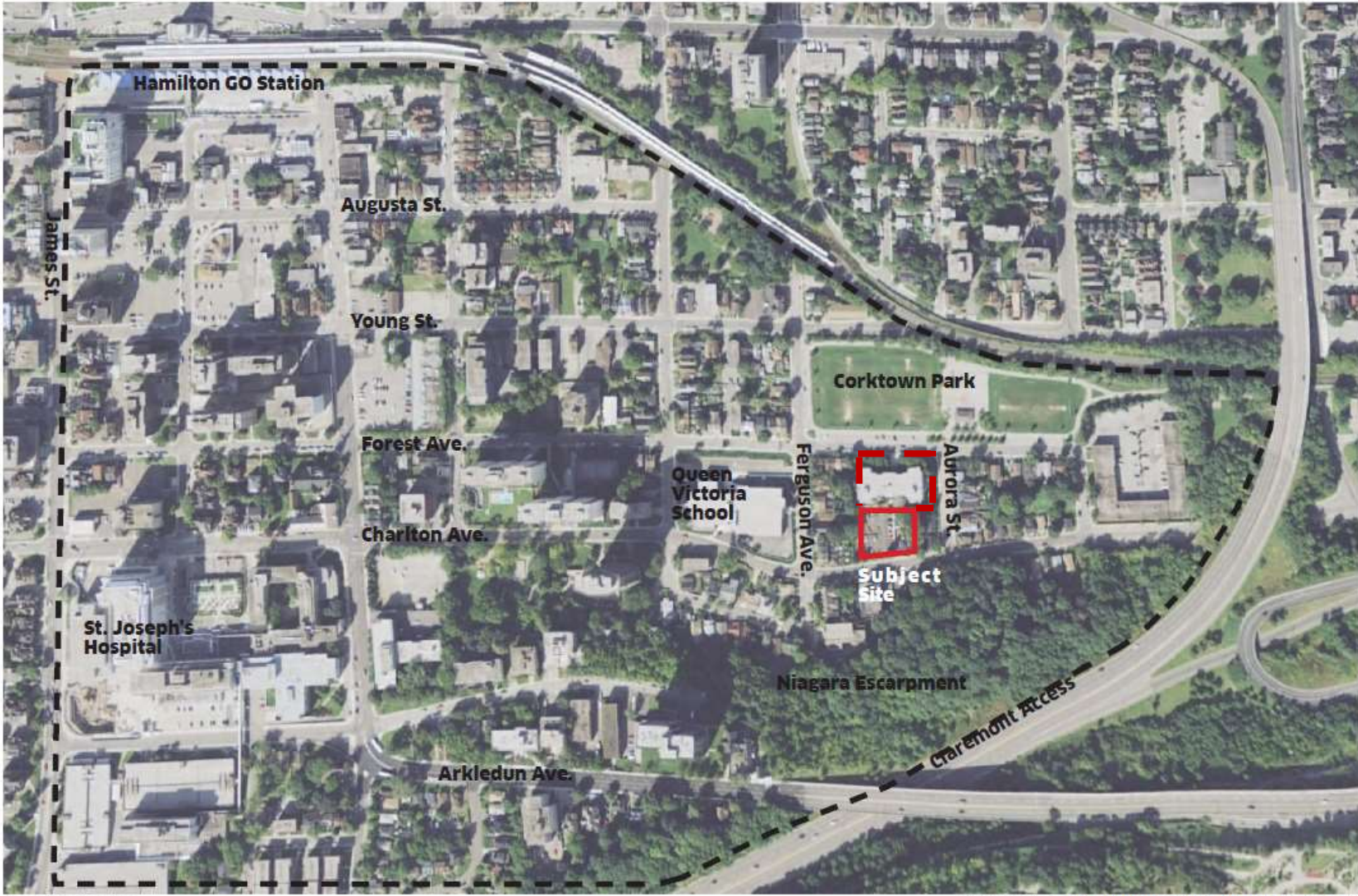


Figure 2: Context Map

■ ■ ■ ■ CORKTOWN NEIGHBOURHOOD BOUNDARY



Figure 3: Map illustrating heights of a selection of surrounding buildings.



200 Chariton Ave East looking South,
Residential Mid Rise Apartment, 6 Floors



200 Forest Ave looking South, Residential Apartment, 4 Floors



182 - 192 Ferguson Ave looking South, Residential Townhouses, 2 Floors



Queen Victoria Elementary School, looking Southwest



193 - 203 Ferguson Avenue South looking East,
Residential Semi and Single Detached, 2 Floors



Ferguson Avenue Pumping Station, looking East



150 Chariton Ave East looking West,
Residential High Rise Apartment, 33 Floors



125 Forest Ave, looking North, Residential Mid Rise Apartment, 9 Floors



Chariton Ave / Wellington St., Looking NE

TRANSPORTATION, CONNECTIONS, AND OPEN SPACE

The proposed development is located within walking distance to the Hamilton GO Bus/Rail Station located approximately 600 metres northwest, as well as Hamilton Transit located approximately 400 metres west along John Street and James Street (Shown in Figure 3).

Currently, the Subject Site is serviced by signaled on-street cycling lanes along Ferguson Ave, Forest Ave, and Charlton Ave providing direct links to the surrounding roads and pedestrian realm.

The southern boundary of the Study Area abuts the Niagara Escarpment, which is part of a regional ecosystem, designated as protected environmental significant natural heritage feature in the Hamilton Official Plan.

The subject site is well served by local neighbourhood parks. Directly to the north of the site is the Corktown Park, a large neighbourhood sized park serving as a central community gathering place and focus for the neighbourhood. Further north from the site is the smaller Shamrock parkette serving local needs.

In summary, the subject site is served by a connected network of cycling and side walks, transit, and parks all within close proximity.

Figure 4: Map of existing transit, cycling, and open space.





View north towards Corktown Park

03 // UHOP & urban design policy

URBAN DESIGN OUTLOOK

This Design Brief is to aid the City in guiding the urban design and site planning for the Subject Site as it relates to the City's urban design and site plan guidelines, as well the city-building objectives envisioned within the Urban Hamilton Official Plan (UHOP). A key urban design principle reinforced throughout the UHOP is the commitment to create compact, interconnected, pedestrian oriented, and transit-supportive communities within which people can attain a high quality of life and make efficient use of lands served with a high-level of existing or planned transit, developed with a mix of uses, and appropriate densities to support transit, walking, and cycling.

The Subject Site is set within Corktown, which plays a significant role as a major urban neighbourhood and historic area within the city. The following section addresses built form, height, massing, and public realm elements through the use of the City of Hamilton Urban Design policies as outlined in the UHOP, the City Site Plan guidelines, and general urban design principles that will ensure that the future intensification of the Subject Site is accomplished in a manner that respects the City's urban design objectives and development policies. The referenced policies are substantially contained within Chapter B – Communities, of the UHOP.

The Subject Site is designated 'Neighbourhoods' in the UHOP. The Official Plan directs that the existing character of designated Neighbourhoods areas shall be maintained. Residential intensification within these areas shall enhance and be compatible with the scale and character of the existing residential neighbourhood in accordance with Section B.2.4 – Residential Intensification and other applicable policies of the UHOP.

This Brief discusses a number of urban design policies, accompanied by a narrative about how the form, function, and design of the Subject Site relates to the urban design objectives of the City and project fit into the urban environment.

The following section describes the general intent and relevance of existing urban design policies, while a more detailed discussion how the proposed development relates to specific City of Hamilton Site Plan Guidelines is included in Part 4.0 of this report.

3.1 URBAN HAMILTON OFFICIAL PLAN:

The site is designated 'Neighbourhoods' in the Urban Hamilton Official Plan. This designation permits a variety of residential types, with building forms ranging from ground oriented single-purpose to multi-storey mixed-use buildings. The City supports infill development and other intensification within the Neighbourhood Designation, provided that it enhances and complements the existing character of established Neighbourhoods. Policy 3.2.4.1 establishes the promotion of development through residential intensification and new development.

Policy 3.2.4.1: "The development of a full range of housing forms, types, and densities shall be provided for and promoted throughout the City of Hamilton through residential intensification and new development. A full range of housing forms, types, and densities means the full spectrum of physical housing types including single detached dwellings, semi-detached dwellings, duplexes, townhouses of various types (street, block, stacked), apartments and other forms of multiple dwellings, and lodging houses, built at a range of densities."

Further, the general residential intensification criteria is more specifically established in Policy 2.4.1.4, including:

- the relationship of the proposal to existing neighbourhood character so that it maintains, and where possible, enhances and builds upon desirable established patterns and built form;
- the development's contribution to maintaining and achieving a range of dwelling types and tenures;
- the compatible integration of the development with the surrounding area in terms of use, scale, form and character. In this regard, the City encourages the use of innovative and creative urban design techniques;
- the development's contribution to achieving the planned urban structure as described in Section E.2.0 – Urban Structure;
- infrastructure and transportation capacity; and,
- the ability of the development to comply with all applicable policies.

The proposed development responds well to Policy 3.2.4.1 and Policy 2.4.1.4 with respect to residential intensification. The proposed 3-storey 'townhouse'/apartment project is designed to fit within the existing mixed residential character of the neighbourhood in form, height, architectural style, while providing a diversity of dwelling types and tenures. In addition, the proposed development intensifies an underutilized lot in close proximity to the Downtown Urban Growth Centre, with existing infrastructure, neighbourhood amenities, and transit available to service the development.

The proposed development also responds well to the more specific intensification criteria related to the Neighbourhood designation established in Policy 2.4.2.2 including:

- the matters listed in Policy B.2.4.1.4;
- compatibility with adjacent land uses including matters such as shadowing, overlook, noise, lighting, traffic, and other nuisance effects;
- the relationship of the proposed building(s) with the height, massing, and scale of nearby residential buildings;
- the consideration of transitions in height and density to adjacent residential buildings;

- the relationship of the proposed lot(s) with the lot pattern and configuration within the neighbourhood;
- the provision of amenity space and the relationship to existing patterns of private and public amenity space;
- the ability to respect and maintain or enhance the streetscape patterns including block lengths, setbacks and building separations;
- the ability to complement the existing functions of the neighbourhood;
- the conservation of cultural heritage resources; and,
- infrastructure and transportation capacity and impacts.

The proposed development conforms to Policy 2.4.2.2 by being contextually appropriate to the existing architectural context. The development is compatible with nearby heritage buildings by protecting significant views, minimizing shadow impacts, and through the use of similar materials and architectural elements characteristic of the surrounding heritage features.

In terms of built form and massing, the proposed development establishes heights that fit within the existing and emerging architectural context of the area, with narrow residential bays, adequate setbacks set close to the street, and consideration for the design of the ground level details. In addition, the site is designed with high quality building materials and architectural design treatments, with consolidated vehicular access, servicing, and loading facilities.

SECTION B3.3 URBAN DESIGN POLICIES

There are no specific site, secondary plan, or urban design guidelines for the Corktown area that contain directions for the development of new buildings as part of a comprehensive vision for the area. Rather, the Urban Hamilton Official Plan and the City of Hamilton Site Plan Guidelines provide urban design guidance for site and building design city-wide.

INTENTION OF SECTION B3.3

The Urban Design Policies are intended to direct appropriate development with a focus on enhancing the sense of community in creating unique places, creating high quality places in private and public development, creating pedestrian oriented environments, transit-supportive, compatibility, promoting development that respects natural features and sustainability, and the promotion of development intensification. Further, Policy 3.3.2 states that the “successful integration of new development and redevelopment in the urban area and its integration with surrounding neighbourhoods requires the form of development to follow appropriate urban design principles. Every design direction will not apply in all situations.”

URBAN DESIGN PRINCIPLES

Policy 3.3.2.4 Quality spaces: “Physically and visually connect the public and private realms. Public and private development and redevelopment should create quality spaces.” The proposed development provides a built form that is consistent with the existing built fabric of the surrounding buildings in height and massing. The building is set close to the street, which is consistent with the adjacent buildings in creating a homogeneous urban edge. In addition, the development includes transitional areas between the private and public realm through the use of front porches, and landscaping that reflects the surrounding heritage character.

Policy 3.3.2.5 Safe, Accessible, and Connected: “Places that are safe, accessible, connected and easy to navigate shall be created by using the following design applications, where appropriate.” The proposed development is designed to relate well to the street, with narrow residential bays, each with direct access to the street. This will have a direct net benefit to the pedestrian-level activity over the existing surface parking lot on site today, through providing a permeable facade and continuity to the urban fabric along the street. In addition, the development provides clearly defined private amenity spaces interior to the block, identifiable site access and parking entrances, with unobstructed access from the street, and a

network of pathways separating the vehicular and pedestrian flows.

Policy 3.3.2.6 Compatibility with the surrounding areas:

“Where it has been determined through the policies of this Plan that compatibility with the surrounding areas is desirable, new development and redevelopment should enhance the character of the existing environment.

“The proposed development complements and animates the existing neighbourhood context through the design of a stacked apartment built form that reflects the surrounding brick masonry characteristic of the adjacent designated heritage features located along Ferguson Avenue. The 3-storey building complements the existing pattern of development and the nature of the surrounding 4 to 6-storey apartment developments directly to the north and south of the proposed building.

Policy 3.3.2.7 Adaptable in accommodating future change:

“Designing buildings, sites, and public spaces that can be used for a variety of uses in the future in response to changing social, economic, and technological conditions; encouraging design that accommodates the changing physical needs of people and their lifestyles through all stages of their lives; and encouraging innovative design of built forms and public spaces.”

The proposed development includes a rental co-op housing project designed for a range of family sizes and demographics. The units proposed include flex-5-bedroom units for the ‘townhouse’ component of the building in order to provide flexibility in accommodating a wider range of family sizes. The first, second and third storeys will house seniors rental units thereby providing housing diversity for a range of lifestyles and stages of life in the same building.

In addition, the proposed development will include outdoor amenity space at the roof level that will help to integrate the residential apartment building to the north with a new and interesting open space accessible to both buildings.

Policy 3.3.2.8 Promote environmental sustainability: “Achieve compact development; integrating, protecting, and enhancing environmental features and landscapes, including existing topography, forest and vegetative cover, green spaces and corridors through building and site design; encourage on-site stormwater management; encourage LEED and reduced containments; and encourage energy efficiency.”

The proposed development minimizes the development footprint through a compact development form and height that matches other existing buildings in the area. The buildings are set close to the Street with enhanced amenity green space, and below grade parking limiting impermeable paved surfaces, and reducing the “heat island” effect over large paved areas.

Policy 3.3.2.10 Transportation Network: “Streets shall be designed not only as a transportation network but also as important public spaces and shall include, adequate and accessible space for pedestrians, bicycles as well as transit, vehicles, and utilities; continuous sidewalks; landscaping such as street trees and landscaped boulevards; pedestrian amenities such as lighting, seating, way-finding signage, and urban braille; on-street parking; public art; and, amenities and spaces that encourage pedestrian activity and animate the streetscape such as public gathering places, patios, and sidewalk cafés”.

The proposed development is designed to relate well and integrate seamlessly with the surrounding public realm. The use of front porches, and the building set close to the street will provide an enhanced transition between the private and public realms. The site is designed to enhance and animate the Charlton Avenue E frontage as a significant east-west corridor through the area with appropriately scaled trees, windows, lighting, and in creating a more comfortable pedestrian environment.

Policy 3.3.3 Built Form: “New development shall serve to maintain and support existing character, or create and promote the evolution of the character in areas where transformations are appropriate and planned.”

Policy 3.3.3.2 Built Form: “New development shall be designed to minimize impact on neighbouring buildings and public spaces through creating transitions in scale to neighbouring buildings; ensuring adequate privacy and sunlight to neighbouring properties; and minimizing the impacts of shadows and wind conditions.”

The proposed 3-storey building represents an appropriate height transition between the existing 6-storey building to the south and the 4-storey building to the north. The proposed building is set close to the street to reduce the shadow impacts to the north, and includes a step back at the third floor to reduce the overall impact along Charlton Avenue at street level.

Along the western property line, a setback will be provided to reduce building mass impacts for the existing low-rise residential buildings along Ferguson Avenue. In addition, the proposed driveway and landscape strip will provide a total of 7.5-metres of separation between the western limit of the adjacent property to the east and the proposed development.

Policy 3.3.3.5 Built form: “Create comfortable pedestrian environments by:

- locating principal façades and primary building entrances parallel to and as close to the street as possible;
- including ample glazing on ground floors to create visibility to and from the public sidewalk;
- including a quality landscape edge along frontages where buildings are set back from the street;
- locating surface parking to the sides or rear of sites or buildings, where appropriate; and,
- using design techniques, such as building step-backs, to maximize sunlight to pedestrian areas.”

The proposed development is intended to achieve a harmonious relationship to the existing built form context in the neighbourhood through building height, massing, setbacks, roof and profile, architectural expression, as well as sensitively integrated vehicle access and loading. The proposed development conforms to this policy by establishing heights that fit within the existing and emerging architectural context of the area, consideration for the design of the lower stacked townhouses cornice lines and the 1.5 metre step-back at the third storey, articulates the façade and reduces the impacts of shadows, and improves sunlight penetration (See Shadow Study in Section 4.7).

The development is designed with high quality stone and masonry materials, and architectural design treatments characteristic of the surrounding designated heritage features. The proposed facades are designed with an appropriate proportion of glazing and openings for improved visibility along the Charlton Avenue frontage.

The building is set close to the street to create a consistent urban edge, and to create a comfortable interface with the public boulevard through a 4.5 metre setback from the front property line. This setback provides adequate space for front yard landscaping, with parking located below grade and to the rear of the building.

Policy 3.3.7.1 Storage, Service and Loading Areas: “Locate and access parking, loading, service areas, and utilities primarily from a lane or at the rear of the building so as not to conflict with pedestrian-oriented activities.”

The proposed development conforms to this regulation by providing loading and servicing functions to the rear of the building. The loading facility will be screened through a combination of landscaping and enclosure around the loading facility.

Policy 3.3.9 Access and Circulation and 3.3.10 Parking: Parking is located to the rear of the building within a below grade structured parking facility to reduce the visual impact, and to maximize the areas available for landscaping and minimize expanses of paved areas.

3.2 URBAN HAMILTON OFFICIAL PLAN: CULTURAL HERITAGE RESOURCES POLICIES:

The Urban Hamilton Official Plan acknowledges that redevelopment will occur in older areas of the City and the need to sensitively integrate development into the urban environment. The UHOP also indicates that the individual heritage properties and areas of heritage value will be conserved by ensuring that new development respects and reflects the design of surrounding heritage buildings.

Policy 3.4.3.6: “The City shall protect established historical neighbourhoods, as identified in the cultural heritage landscape inventory, secondary plans and other City initiatives, by ensuring that new construction and development are sympathetic and complementary to existing cultural heritage attributes of the neighbourhood, including lotting and street patterns, building setbacks and building mass, height, and materials.”

The subject redevelopment area is mainly vacant, except for two frame houses at 221 and 223 Charlton Avenue E, which are listed buildings in the City of Hamilton's Heritage Register Volume 2 for Historical and/or Architectural Interest and Volume 3 for the Canadian Inventory of Historic Buildings. As well, the site is adjacent to 207, 211, 213, and 215 Ferguson Avenue South which have facade, opening, and window features designated under Part IV of the Ontario Heritage Act.

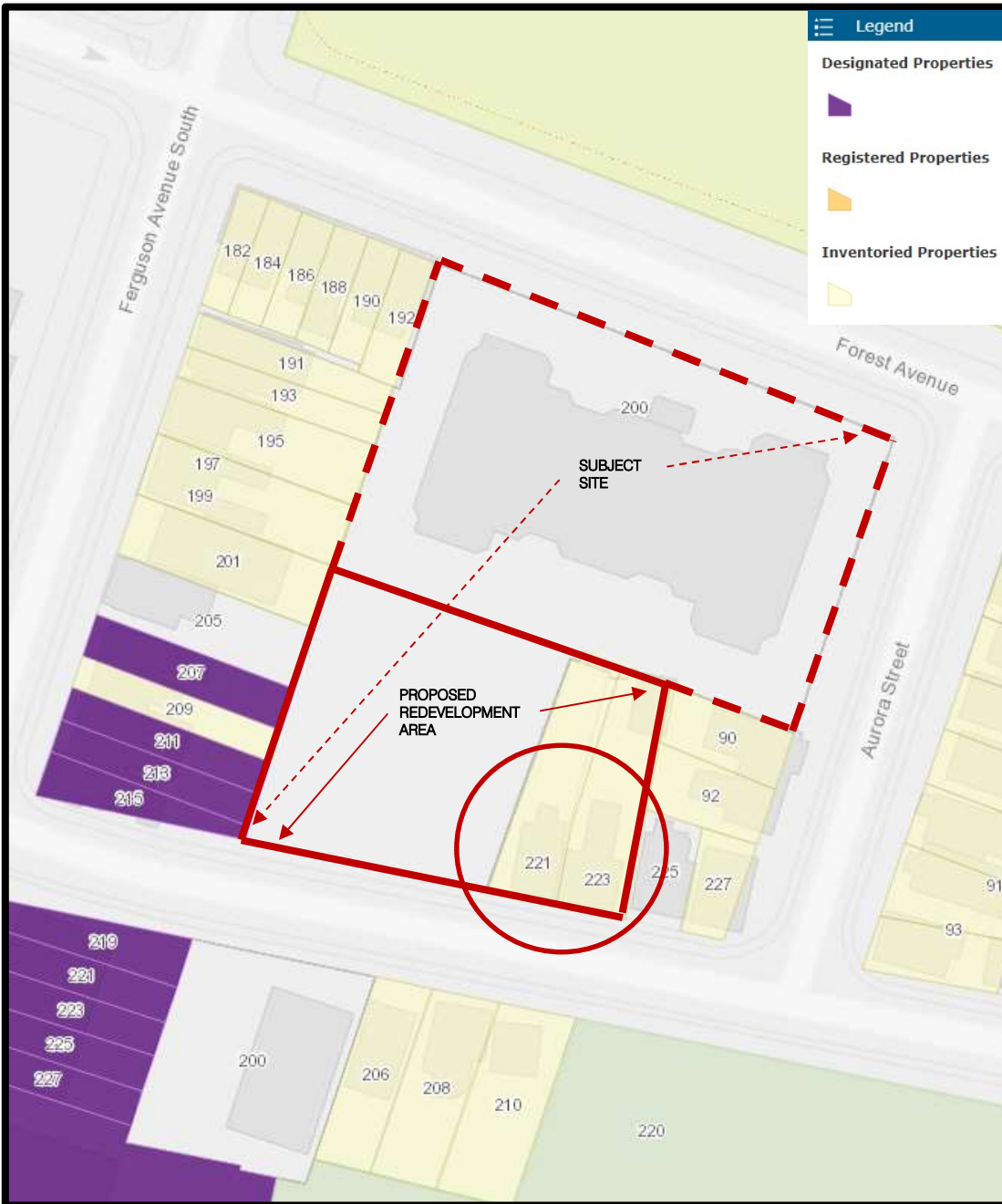
2.1 SCOPED ASSESSMENTS

a) Where it has been adequately demonstrated that the conservation, rehabilitation and reuse of cultural heritage resources that have not yet been included in the City's Municipal Heritage Register, nor designated under the Ontario Heritage Act, is not viable, the City may require that the affected resources be thoroughly documented for archival purposes at the expense of the applicant prior to demolition or removal.

b) Where cultural heritage resources may be affected and staff is of the opinion that the potential impacts will be minor, the discussion of impacts to cultural heritage resources may be integrated into an Urban Design Brief or Urban Design Study.

Approach to reducing impacts to adjacent heritage resources:

The design of the proposed building at 200 Forest Avenue does not intend to replicate historic design, but rather provides a contemporary design solution, which is respectful and compatible with the adjacent heritage features. The building is designed with attention to form, alignment, rhythm of residential bays, massing, setbacks, architectural features, colour schemes, and materials. (cont'd next page)



Inventoried Property
 Address: 221/223 Charlton Ave E, Hamilton
 Heritage Date: 1860
 Architect/Builder:
 Original Owner:
 Preliminary Design Value:
 Preliminary Associative Value:
 Preliminary Contextual Value:
 Classification:
 Inventory Type: LACAC, CIHB, HMHC
 Survey Date: 2019

Approach cont'd:
 The two frame houses at 221 and 223 Charlton Avenue E (see Figures this page) have been *inventoried* but not yet *registered* in the City's Municipal Heritage Register, nor *designated* under the Ontario Heritage Act. It is not viable to maintain these houses within the context of the proposed redevelopment of the Subject Site.

Building Materials: The basic materials, details, and openings reflect the materials expressed in other historic Corktown architectural features. The proposed masonry patterns as well as the details reflect the patterns expressed within adjacent Ferguson Avenue properties.

The materials proposed for the development are based on the masonry traditions of the area. The facades are composed of the traditional masonry elements with window and door openings that respect the traditions of masonry design. The windows are detailed to establish the sense of scale appropriate to surrounding character. The stone and/or cast stone details respect and build upon some of the detail found in the heritage buildings in the surrounding area.

Streetscape: The proposed development is set close to the front property line to maintain the historic streetscape 'look and feel' found throughout Corktown. The horizontal elements, articulations, recesses, and openings are designed to be proportional with the building design, and as expressed in the heritage facades of the row houses fronting along Ferguson Avenue and throughout the Corktown neighbourhood. These architectural features are intended to add to the overall unity of the street.

Rhythm of Facades: The facades have been designed with both an underlying rhythm and a proportioning system. The facades have been composed by a narrow rhythm of symmetrical residential bays (approximately 6 metres wide) consistent with the adjacent heritage row houses. The residential bays are ordered by an underlying rhythm and governing lines manifested by a string of horizontal sill features and lines, which reference the architectural style of the adjacent heritage buildings along Ferguson Avenue.

At-grade level: The at-grade townhouse residential facade design is based on the general character of the district and the correct proportions of the building. Each proposed townhouse is designed with a distinctive porch entrance reflective of the covered porches found throughout the neighbourhood.

Above-grade levels: The design and composition reflects the local symmetries, scale, and rhythms as expressed in the adjacent heritage features. The typical windows are mostly paired sash within the masonry openings. The actual proportion used is generally consistent with the openings of the adjacent heritage features.

Height and Massing: The building height and massing has been designed to be proportional to the site and to the overall neighbourhood context. The proposed 3-storey building is set between a 6-storey building to the south and a 4-storey building to the north. The adjacent row houses to the west along Ferguson Avenue are 2 storeys.

The 3-storey height of the proposed development relates well with the existing heights in the neighbourhood. The built form, materials, and architectural quality expresses the qualities of the surrounding heritage features and respects the spirit of the design context found in the area. The highest portions of the project are set back from the street with step-back at the third storey respective of an angular plane thereby reducing the overall building height effect at the street level.

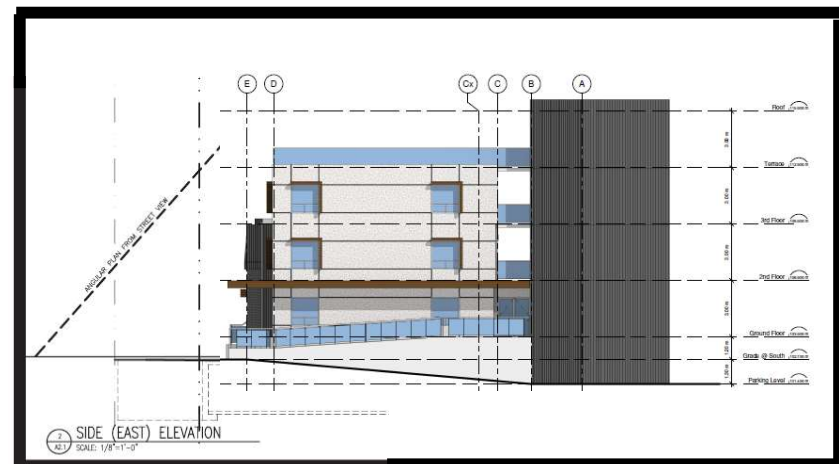
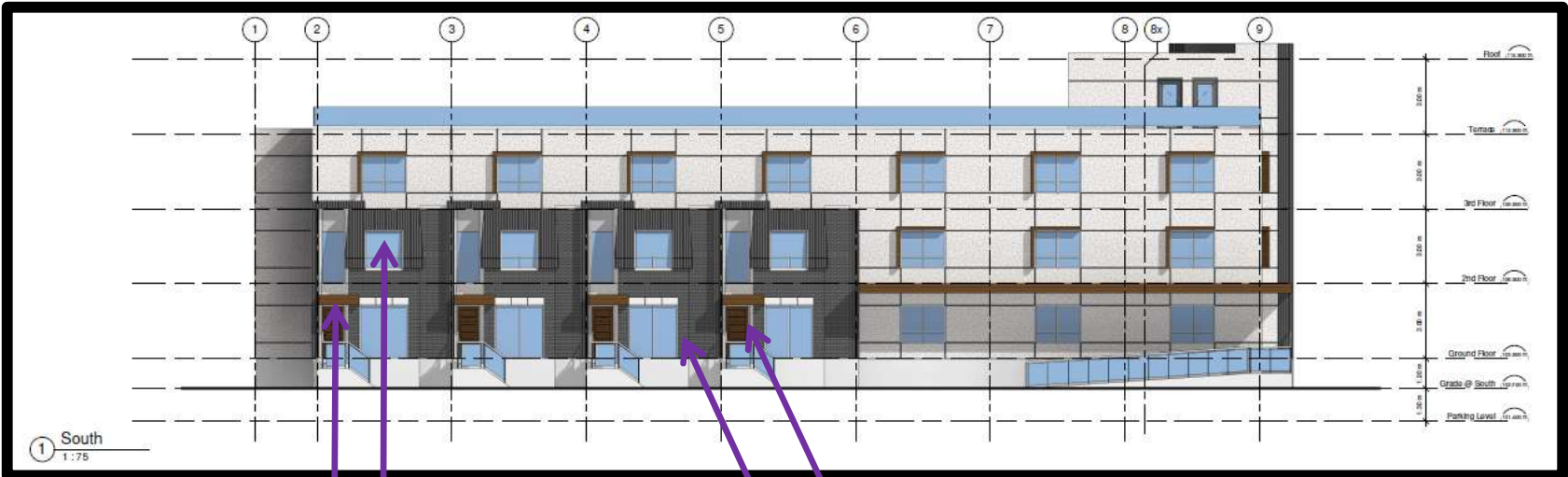


Figure 5 - Setbacks and Angular Plane



The facades have been composed by a narrow rhythm of symmetrical residential bays (approximately 6 metres wide) consistent with the adjacent heritage row houses.

Horizontal sill features.

Distinctive porch entrance reflective of the covered porches found throughout the district.

The proposed masonry patterns as well as the details reflect the patterns expressed within adjacent Ferguson Avenue properties.

Figure 6 - Approach to Reducing Impacts to Adjacent Heritage Resources

04 Hamilton site plan guidelines

4.0 PROPOSED DEVELOPMENT AND FUNCTIONAL SITE PLAN REQUIREMENTS

The following section describes the proposed development components as they relate to the City of Hamilton Site Plan Guidelines. The intent of this section is to express how the proposed development expresses the City's preferences and expectations for site development. Where the project does not meet a specific City Site Plan development standard a rationale will be provided for the alternative solution.

4.1 BUILDING FIT

Policy 4.6.1: "New building design should complement established neighbourhood character through consideration of the following:

- new buildings should be scaled to existing adjacent structures;
- existing setbacks and building heights should be respected in determining an appropriate setback and height of new buildings;
- the proportions and elements of existing buildings should be used where possible to determine an appropriate relationship for new buildings;
- roof profiles, windows, entrances and porches that are predominant within the streetscape should be considered in the design of new buildings; and,
- where possible, materials of new construction should be selected from the variety of materials found within the existing neighbourhood."

Neighbourhood Development Patterns: The neighbourhood surrounding the subject property is transforming to include taller buildings, including the buildings to the north and south of the site. Further west along Charlton Avenue in particular, blocks include higher density tall building forms. Please refer to Section 2.2 Neighbourhood Context for examples of higher density and taller buildings in the area.

The proposed 3-storey building is designed with architectural materials, form, massing, and orientation that is consistent with the surrounding built form and neighbourhood context

4.2 BUILT FORM AND MASSING

Policy 4.2 Siting Buildings in a Neighbourhood: "Within the city, there are properties that because of their location can be used to create an interesting feature building along the street."

Policy 4.2.1: "Buildings should be sited to frame streets and terminate vistas. Attention should be placed on the massing of the building and the building detailing to create a landmark structure."

Policy 4.4 Massing and Building Design: "The massing of a building and the design of a building façade can create an attractive development and edge to the street. Building exteriors which face a street form part of the public realm and can affect the use and perception of adjacent pedestrian areas. Well designed buildings will enhance pedestrian use along the street and create a strong sense of neighbourhood pride."

Policy 4.4.1: "The principal building facades should be oriented toward the public street and not the parking lots or other areas."

Policy 4.4.3: "The main facades should have sufficient translucent glazing to provide casual surveillance of outdoor areas."

Policy 4.4.4: "The main entrances to a building should be emphasized through the use of canopies and other treatments that will provide both visual identification as well as weather protection for pedestrians."

Policy 4.4.6: "Building design should break up large building facades at street level and avoid flat or blank walls. Where large sections of blank walls are unavoidable, architectural techniques such as modulation, display windows, textures and colour changes can be used to enhance the elevation."

Defined Urban Edge: The proposed development is designed to create a defined urban edge by setting the building close to the street, with enhanced architectural quality and features to create a strong pedestrian level environment and street enclosure.

The building is provided with multiple grade level entrances and windows to create a more permeable building edge and a perceivably safer urban environment. The symmetry and rhythm of residential bays breakup the building to avoid blank

homogeneous walls and reflect the rhythm expressed within the heritage buildings along Ferguson Avenue.

Form and Massing: The design of the proposed building provides a contemporary design treatment to an existing and emerging architectural context of a mix of building forms in the area. The massing of the lower two storey 'townhouses' responds well to the adjacent heritage architecture, with the three storey apartment transitioning from the building edge at the front. The building is also generally consistent with the heights of adjacent buildings.

Policy 4.3 Microclimate Design: "Building size, height, and placement on a site all have an impact on how the pedestrian space and adjacent properties are used. Possible impacts include wind levels at grade, shadow casting, and snow loads. The location and size of buildings on a site should be such that they minimize the adverse impacts on the microclimate of the adjacent buildings, streets, and the comfort level of pedestrians using open areas of the site."

Transitions: the proposed development establishes an effective transition and overall balance of building height with surrounding buildings. Specifically, the proposed building stair-tower above the third storey will be setback from the building edge to the rear of the building, establishing an effective transition from the public right-of-way.

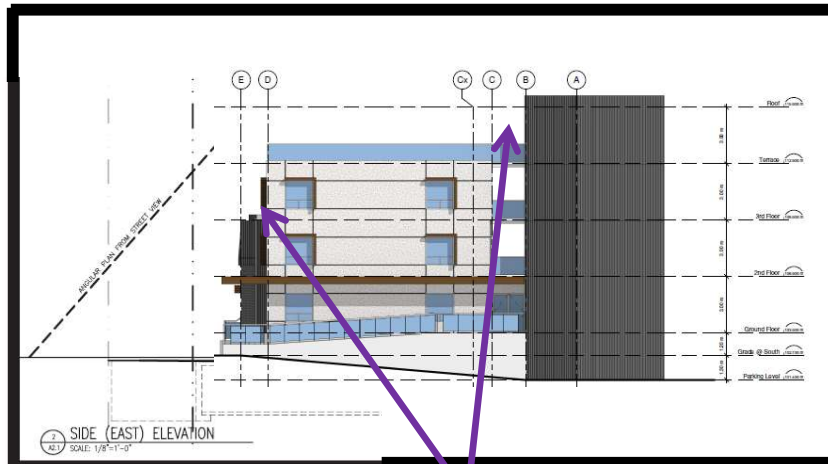


Figure 7 - Transition

Step-back at 2nd Floor and additional Step-back at 3rd

Appropriate Mitigation of Height and Mass: The proposed development establishes a building design that mitigates the effects of height and the perception of mass as the building rises.

This is achieved through the use of step-backs, landscaping, and horizontal and vertical articulation of architectural features and windows.

4.3 SITE ACCESS AND CIRCULATION

Policy 3.2 Site Circulation: "The design of the site circulation system should support the City's streetscape objectives and provide for the needs of both pedestrians and motorists. Safe and direct routes should be provided with priority given to the pedestrian."

Policy 3.2.1 Vehicular Circulation and Parking: "The number of driveway connections to the public street should be minimized to strengthen streetscapes. Mutual or shared driveways are encouraged, especially along collector and arterial roads, to minimize the number of driveways."

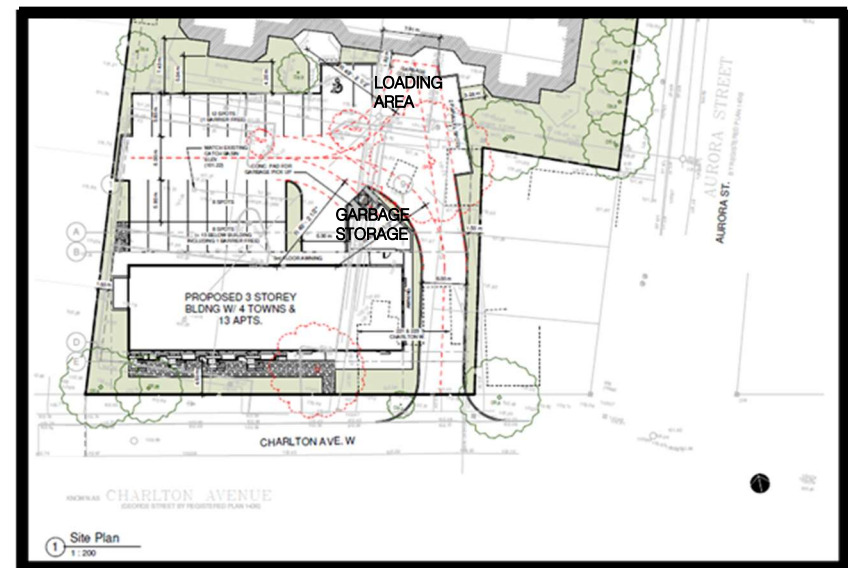


Figure 8 - Vehicle and Pedestrian Circulation and Parking

4.4 LANDSCAPE DESIGN AND AMENITY SPACE

“Parking areas should generally be located behind the building mass at the side and rear of buildings. This will allow buildings to be located closer to the street and reinforce the City’s objective of creating attractive, pedestrian-oriented streets.”

Parking: The site is designed to reduce site circulation and increase pedestrian safety through limiting at-grade parking and providing residential and visitor parking stalls below grade with a small portion provided at-grade to the rear of the building. No parking will be between the front facade of the building and the property line to create a more pedestrian-oriented site and public streetscape. Given the small site area, parking will inherently be located close to consolidated entrances and provide barrier-free access.

The Site will be serviced by one, 6.0-metre one-way driveway with access from Charlton Ave E. Parking stalls will be provided at the rate of 43 parking spaces on the lower level. There are 2-BF parking space and a loading space on the lower level.

Policy 3.2.3 Parking Structures: “Vehicular entrances and exits should be sited to minimize conflicts with pedestrians and provide safe intersections with public streets.”

Pedestrian Access: The site is designed to reduce vehicular circulation at-grade to increase grade level landscaping and pedestrian safety and flow. The site will be built close to the public right-of-way with direct access to residential units from the street. There is a walkway along the ingress driveway between the driveway and the building, which provides access from the front of the building to the rear raised parking structure through the east stairway/elevator lobby.

Policy 3.3 Landscape Design: “Landscape treatments can be used to frame and soften structures, define spaces and screen undesirable views. Neighbourhood character and continuity can be created through front yard landscaping. The landscape design should be integrated with buildings and surrounding features to enhance the aesthetics and function of both the site and the neighbourhood. Hamilton places an emphasis on streetscapes and front yard landscaping treatments.”

Policy 3.3.1: “Landscaping should consider and reflect established neighbourhood landscape character. Front yard landscaping should be compatible with adjacent properties along the street and result in a positive impact on the street.”

Policy 3.3.1: “The scale and function of landscape materials should be appropriate for the site and its structures and to maintain a pedestrian scale. Trees should be selected and placed according to the height and spread they will achieve at maturity.”

Landscaping and Streetscape:

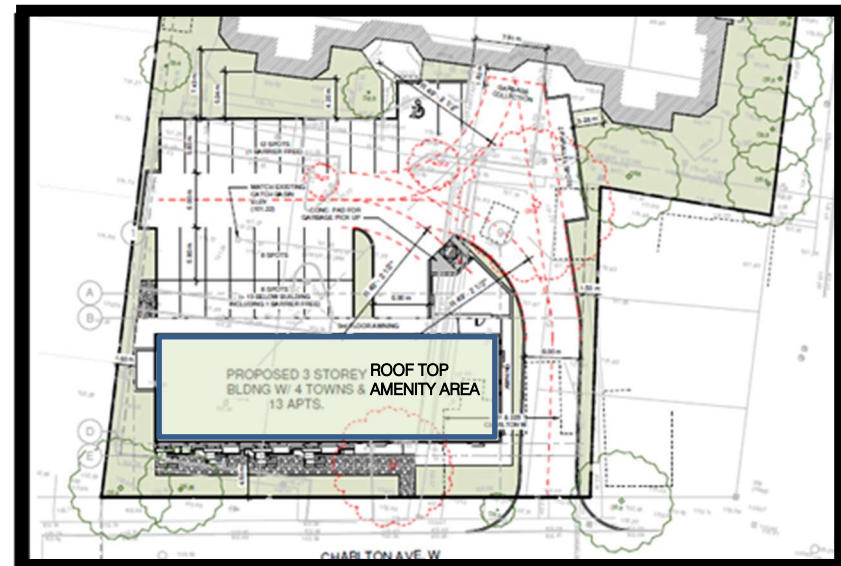


Figure 9 - Landscape Areas and Amenity Space

Policy 3.3.1 Amenity Areas: “Outdoor furniture and fixtures such

as special lighting, trellises, arbours, raised planters, benches and fencing should be considered where people will congregate in outdoor spaces.”

Outdoor Amenity Space: The proposed development includes approximately 500 square metres of outdoor roof-top amenity space provided for the residents of the building. The amenity space will provide a space for a planted herb garden.

4.5 LOADING, STORAGE AND UTILITY AREAS

Policy 3.5.1 Loading bays, and other service areas should be oriented away from public street views and preferably screened from the street by building mass.”

The proposed development will include loading space located at-grade to the rear of the property, in conformity with the requirements of Zoning By-Law. A day bin storage area and a garbage compactor are provided adjacent to the Type B loading space, with additional residential garbage and storage rooms are provided within the below grade parking structure.

4.6 BUILDING MATERIALS AND ARCHITECTURAL TREATMENT

The architectural treatment seeks to reference traditional heritage character of the area with robust stone materials forming a rusticated base, masonry materials forming the main body of the walls and various parapet, coping and cornice treatments to define building caps. This traditional facade composition echoes the patterns of the tradition historic buildings within Corktown.

The proposed building design seeks a vertical patterning using masonry articulations and openings that establish a repetitive bay width that is reflective of traditional residential forms in the area. The pattern is reinforced with stepping of the street wall to create shadow lines emphasizing corner treatments and other focal points near individual residential entry points.



Figure 10 - Architectural Treatments

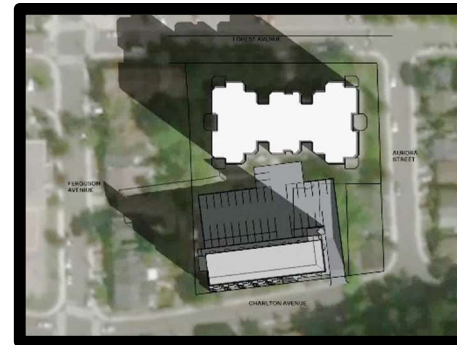


4.7 SHADOW IMPACTS

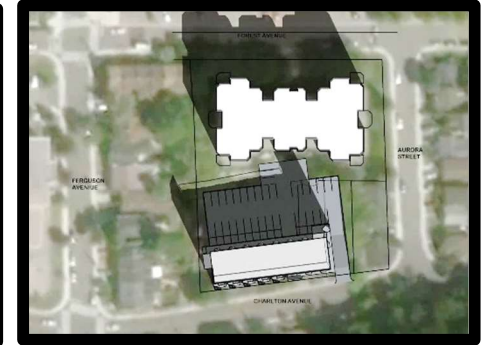
A shadow study was prepared by Romanov Romanov Architects for the proposed development. The study illustrates the incremental ground level shadow impacts during the summer/winter solstice (June 21/December 21) as indicated in the City of Hamilton Site Plan Guidelines. The following paragraphs highlight instances where the proposed development produces new incremental shadow impacts on adjacent properties.

207, 211, 213, and 215 Ferguson Avenue: The proposed development will not result in marginal incremental ground level shadows on the adjacent 207, 211, 213, or 215 Ferguson Avenue properties.

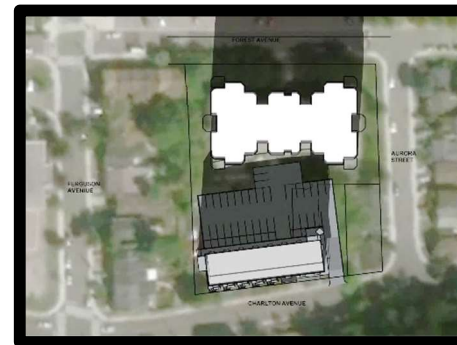
205, 203, and 201 Ferguson Avenue: The proposed development will result in marginal incremental ground level shadows on the adjacent 205, 203, and 201 Ferguson Avenue properties between 10:00 am and 12:00 pm on December 21st.



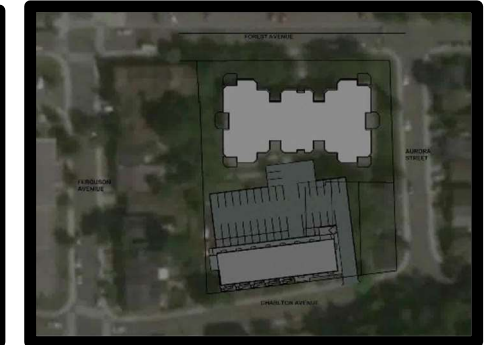
Dec 21 - 10:00 AM



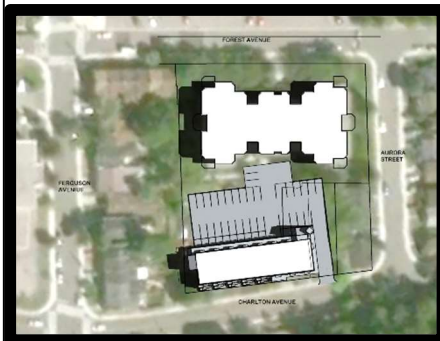
Dec 21 - 12:00 PM



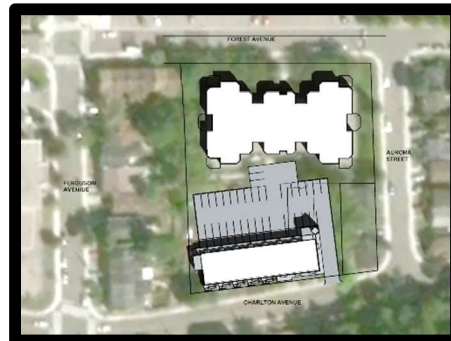
Dec 21 - 2:00 PM



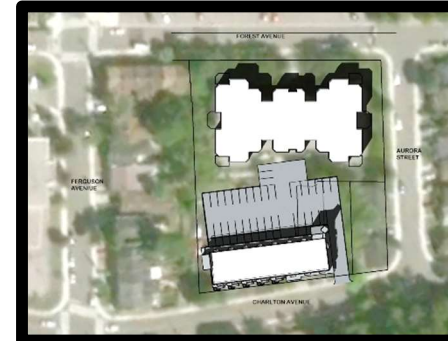
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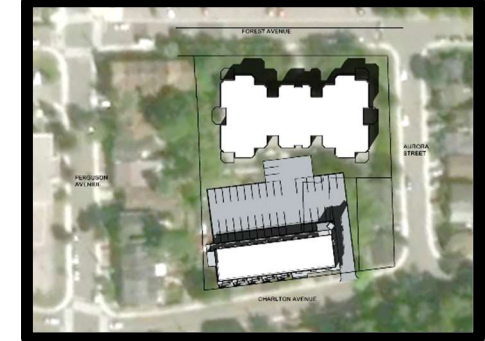
June 21 - 10:00 AM



June 21 - 12:00 PM



June 21 - 1:00 PM



June 21 - 2:00 PM